

**FILED**

APR 13 1994

COUNTY CLERK  
BY *Debra Dubois*

IN THE BOARD OF COUNTY COMMISSIONERS OF LANE COUNTY  
STATE OF OREGON

ORDER NO. 94-2-15-10

(IN THE MATTER OF APPROVING A PROJECT DESIGN  
(CONCEPT FOR IMPROVEMENT OF NORTH 19TH STREET  
(AND YOLANDA AVENUE.

**WHEREAS**, improvement of North 19th St. from the Springfield city limits to Hayden Bridge Road has been approved through adoption of the FY 1993-1994 through FY 1997-1998 Capital Improvement Program; and

**WHEREAS**, Lane Manual 15.580 has established a process for citizen input with regard to individual road improvement projects; and

**WHEREAS**, a public meeting was held on July 15, 1993 at Briggs Middle School to consider improvement of North 19th Street; and

**WHEREAS**, on August 24, 1993, the Roads Advisory Committee reviewed the public meeting record and additional information prepared by County staff, adopted recommendations and findings specifying a design concept for the North 19th St. project including extension of the northern project limit to the intersection of Yolanda Ave. and 23rd St., and directed staff to mail these findings to property owners within the project area as specified in Lane Manual 15.580; and

**WHEREAS**, the recommendations and findings were mailed to property owners within the project area and more than 50 percent of said owners objected within 30 days of their mailing; and

**WHEREAS**, a public hearing was conducted by the Board of Commissioners on February 15, 1994 at Springfield City Hall in accordance with Lane Manual 15.580 to further consider the recommendations and findings of the Roads Advisory Committee; **NOW THEREFORE BE IT**

**ORDERED**, that the Board approves the project design concept identified in Exhibit A for improvement of North 19th St. and Yolanda Ave. between the Springfield city limits and 23rd St. based on the findings also in Exhibit A;

**FURTHER ORDERED**, that staff prepare a right-of-way plan necessary to construct the project; pursue all necessary planning actions; acquire right-of-way and prepare plans and specifications for improvement of North 19th St. and Yolanda Ave. pursuant to this order.

DATED this 12th day of April 1994.

APPROVED AS TO FORM

Date 4-5-94 Lane county

*Stephen D. Baker*  
OFFICE OF LEGAL COUNSEL

*Jerry Rust*  
Chair  
Lane County Board of Commissioners

## Exhibit A

Lane County Board of Commissioners  
Design Concept and FindingsNorth 19th Street and Yolanda Avenue  
Springfield City Limits to North 23rd Street**SELECTED DESIGN CONCEPT**

- 1.) Establish project limits as North 19th St. from the Springfield city limits to Yolanda Ave., and Yolanda Ave. from North 19th St. to North 23rd Street. Amend the Capital Improvement Program to delay the project until FY 1995-1996. Increase Right-of-Way costs to \$110,000 and General Construction costs to \$1,100,000.
- 2.) Purchase the necessary right-of-way to provide a 60' wide right-of-way throughout the project length.
- 3.) Construct a 3-lane urban roadway including a 12' wide continuous shared turn lane, two 11' wide travel lanes, two 5' wide bicycle lanes, concrete curb and gutter, and 7' wide sidewalks along North 19th Street between the Springfield city limits and Hayden Bridge Road. This configuration complies with AASHTO and ODOT design standards.
- 4.) Construct a 2-lane urban roadway including 11' wide travel lanes, two 6' wide bicycle lanes, concrete curb and gutter, and 7' wide sidewalks along the remainder of North 19th St. and along Yolanda Ave. between North 19th and 23rd Streets. This configuration complies with AASHTO and ODOT design standards.
- 5.) By exception to Transplan Implementation Policy I12, construct curbside sidewalks throughout the project length.
- 6.) By exception to Transplan Implementation Policy I9, provide for loading and unloading of busses while parked in the bicycle lane.
- 7.) Provide storm drainage structures and/or systems outside of the road construction limits as required for proper drainage of the new road structure. Coordinate project design with the City of Springfield in order to avoid conflicts with future sanitary sewer construction.
- 8.) Assess the cost of constructing curbs, gutters and sidewalks to the abutting property owners.
- 9.) Install a traffic signal at the intersection of North 19th Street and Hayden Bridge Road.
- 10.) Reconstruct North 19th St. to meet Yolanda Ave. at a 3 legged urban intersection. Build and stripe the intersection to include a left turn pocket for movements from northbound 19th St. to westbound Yolanda Avenue.

**BOARD OF COMMISSIONERS FINDINGS**

- 1.) Establish project limits.
  - a.) Public testimony received at the July 15, 1993 public meeting indicated a significant level of support for providing improvements up to the intersection of Yolanda Ave. and North 23rd Street.
  - b.) Recent pavement ratings for North 19th St. and Yolanda Ave. beyond Hayden Bridge Rd. indicate that major structural repair is needed. Extensive pavement patching was recently completed in order to maintain the roadway until resolution of this proposed project.
  - c.) Accident records indicate that nearly one-quarter of the reported accidents within the five year study period occurred at the intersection of North 19th St. and Yolanda Avenue (within the proposed project extension).
  - d.) Improvements to North 19th St. and Yolanda Ave. are recognized within existing adopted transportation plans (TRANSPLAN).
  - e.) The established project limits will roughly double the total length of improvement as listed in the FY 1993-1994 through FY 1997-1998 Capital Improvement Program. The additional design work will require delay of project construction by one year. The Board agrees that the one year delay is necessary.
- 2.) Purchase 60' wide right-of-way throughout the project length.
  - a.) Construction of 5' bicycle lanes, placement of utility poles at the right-of-way line and utilization of 2 ft. sidewalk cut-outs at utility pole locations will allow construction of the 3-lane portion of the project within a 60' wide right-of-way.
  - b.) The 2-lane portion of the project, even with the recommended 6' wide bicycle lanes will also fit within a 60' wide right-of-way.
  - c.) A 60' right-of-way width will drop the number of property owners from which right-of-way will have to be purchased from 45 to 17. The Board directs that the project right-of-way width should be 60 feet.
- 3.) Construct a 3-lane urban roadway along North 19th Street between the Springfield city limits and Hayden Bridge Road.
  - a.) A 3-lane urban roadway section will address the existing operational problems on this portion of North 19th St. as identified through testimony at the July 15, 1993 public meeting. Specific problems include lack of roadway shoulders, lack of gaps in the oncoming traffic to allow left turns and delays caused by vehicles waiting to make left turns.

- b.) The 3-lane urban roadway section conforms to the implementation policies in TRANSPLAN (Eugene-Springfield Metropolitan Area Transportation Plan) concerning bicycle lanes, sidewalks and center turn lanes on major collector roads.
  - c.) Construction of 5' bicycle lanes, placement of utility poles at the right-of-way line and utilization of 2 ft. sidewalk cut-outs at utility pole locations will allow construction of the 3-lane portion of the project within a 60' wide right-of-way.
- 4.) Construct a 2-lane urban roadway along North 19th Street between Hayden Bridge Road and Yolanda Ave., and on Yolanda Ave. between N. 19th and 23rd Streets.
- a.) Analysis of future development patterns indicates that an additional 300 - 375 dwelling units could be built between North 19th and 31st Streets (north of the EWEB bikepath) within the useful life of these improvements. These additional dwelling units could increase the average daily traffic volume on Yolanda Ave. and North 19th St. by approximately 3,600 vehicles per day. The resulting future total traffic volume would be approximately 10,500 vehicles per day.
  - b.) Two travel lanes will be adequate to accommodate the future volumes calculated in 4.) (a) above at an acceptable level of service. Research studies, however, indicate that a center turn lane does provide a safer roadway. With no turn lane, approximately 11-35% more accidents could occur over time if the conclusions of these studies are applicable to this roadway.
  - c.) In the 6 year period 1987 - 1992, seven accidents occurred in the proposed 2-lane section of roadway. Two of these seven may have been prevented if the proposed intersection improvements at North 19th and Yolanda Avenue were already installed. Based on the studies cited above, the remaining five accidents could be expected to decrease by 1 to 2 accidents over a similar 6 year period if a center turn lane were constructed. All accidents in the proposed 2-lane section were either property damage or minor injury accidents.
  - d.) The City has requested that a 3-lane section be built on both North 19th St. and Yolanda Avenue. The existing city section of Yolanda Ave. east of 23rd St., however, is only 36 ft. in width. The proposed 2-lane section (34 ft.) will match this existing roadway much better than a wider 3-lane option (44 ft.). The Board disagrees with the City assessment of needed lanes.
- 5.) Construct curbside sidewalks throughout the project length.
- a.) Written testimony received at the July 15, 1993 public meeting was generally opposed to the construction of planting strips between the

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- curb and sidewalk. Arguments against the planting strips included elimination of front yard depth in areas where structures are close to the existing roadway and reluctance on the part of adjacent property owners to maintain these landscaped areas. The Board agrees with the public testimony and feels curbside sidewalks should be constructed.
- 6.) Provide for loading and unloading of busses while parked in the bicycle lane.
    - a.) Lane Transit District has indicated that at times dedicated turnouts can be a problem for bus operations since it may be difficult for drivers to find a large enough gap in the traffic flow to re-enter the travel lane. The Board agrees that dedicated bus turnouts within these project limits are not necessary.
  - 7.) Provide storm drainage structures and/or systems outside of the road construction limits as required for proper drainage of the new road structure. Coordinate project design with the City of Springfield in order to avoid conflicts with future sanitary sewer construction.
    - a.) Project extension will require construction or reconstruction of storm drainage systems outside of the road construction limits. Work may include replacement of existing pipe sections which are too small in size to handle the additional runoff. Planning of these storm sewer improvements and the road work should be closely coordinated with the City of Springfield to avoid potential conflicts with future sanitary sewer. This type of coordination is in the interest of both jurisdictions.
  - 8.) Assess the cost of constructing curbs, gutters and sidewalks to the abutting property owners.
    - a.) Section 15.636 (1) (b) of the Lane County Master Road Plan states, "The cost of constructing curbs, gutters and sidewalks within Urban Service Boundaries shall be assessed to the abutting property on a cost per front foot basis. All other costs within Urban Service Boundaries and all costs in the rural areas shall be paid by Lane County."
  - 9.) Install a traffic signal at the intersection of North 19th Street and Hayden Bridge Road.
    - a.) This intersection meets traffic signal warrants number 1 and 8 as specified in Part IV, Section C of the Manual of Uniform Traffic Control Devices (MUTCD).
  - 10.) Reconstruct North 19th St. to meet Yolanda Ave. at a 3 legged urban intersection. Build and stripe the intersection to include a left turn pocket for movements from northbound 19th St. to westbound Yolanda Avenue.

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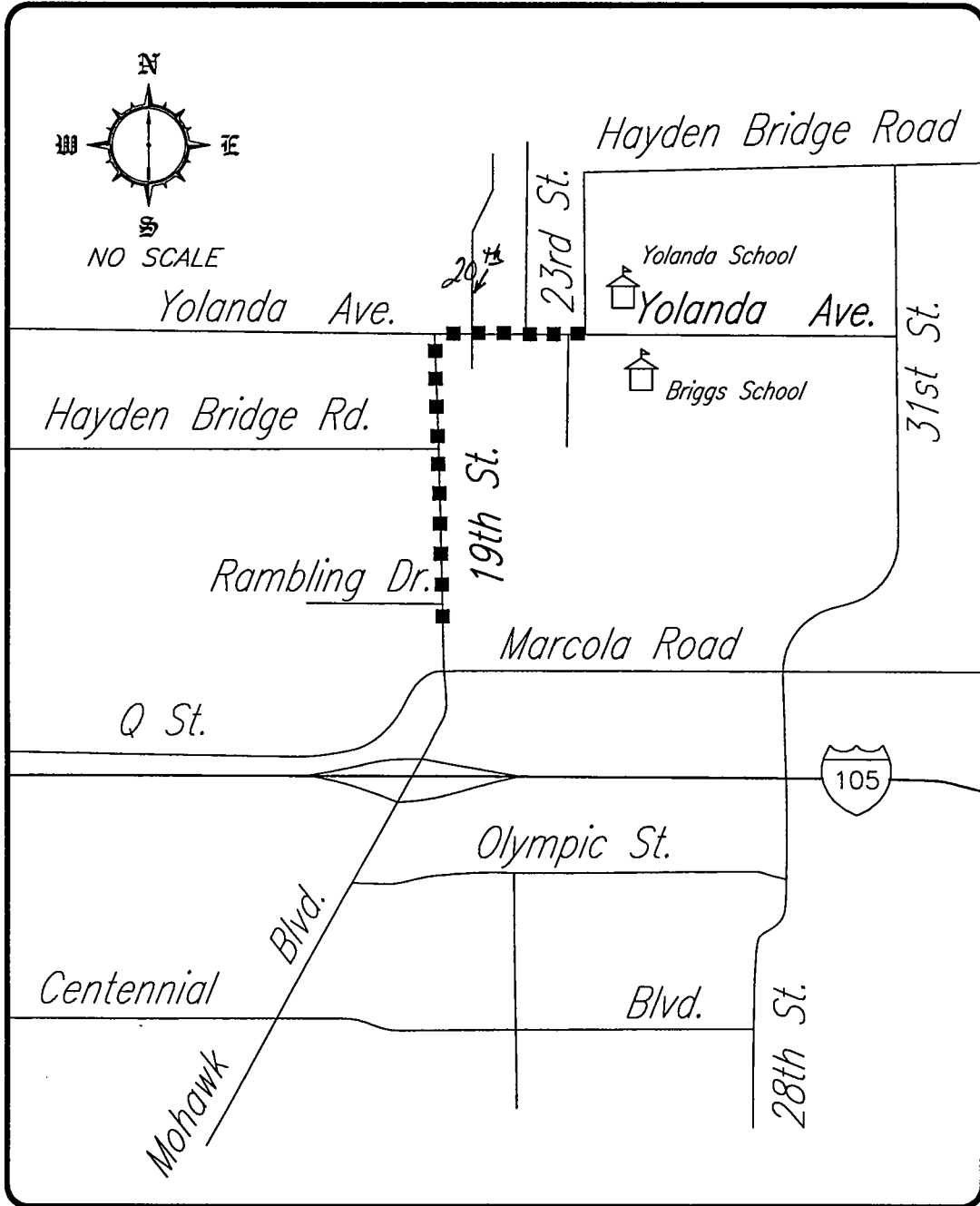
- a.) Nearly one-quarter of the reported accidents within the five year study period occurred at this intersection. In addition, testimony received at the July 15, 1993 public meeting indicated significant operational problems exist at the intersection. Reconstruction will correct turning radius conflict points at the intersection, and grade differentials on Yolanda Ave. east and west of the intersection.
- b.) The new intersection will accommodate future extension of North 19th Street for access to residentially zoned lands north and west of the intersection. The Board believes, however, that this anticipated development will occur far enough in the future to justify only a 3 legged intersection at this time.
- c.) An analysis of intersection capacity at future volumes with and without a northbound left turn pocket has been completed by staff based on procedures in the Highway Capacity Manual. Without the turn pocket the intersection would operate at Level of Service "E" (Scale A-F, F being lowest). With a turn pocket, the Level of Service improves to "D".
- d.) AASHTO and ODOT design standards call for construction to Level of Service "D" or higher on urban or suburban collector roads. TRANSPLAN specifically identifies Levels of Service "E" and "F" as inadequate capacity for planned improvements within the metro area.
- e.) Construction at Level of Service "D" will reduce vehicle delays by providing a refuge for left turning vehicles and allowing free movement of the dominant northbound to eastbound movement. The proposed left turn pocket is also consistent with future extension of North 19th Street. The Board agrees with the staff report and directs that the left turn pocket be included in the project.

#### **ATTACHMENTS**

- Attachment 1: Project Vicinity Map.  
Attachment 2: 2-lane and 3-lane urban roadway typical sections.



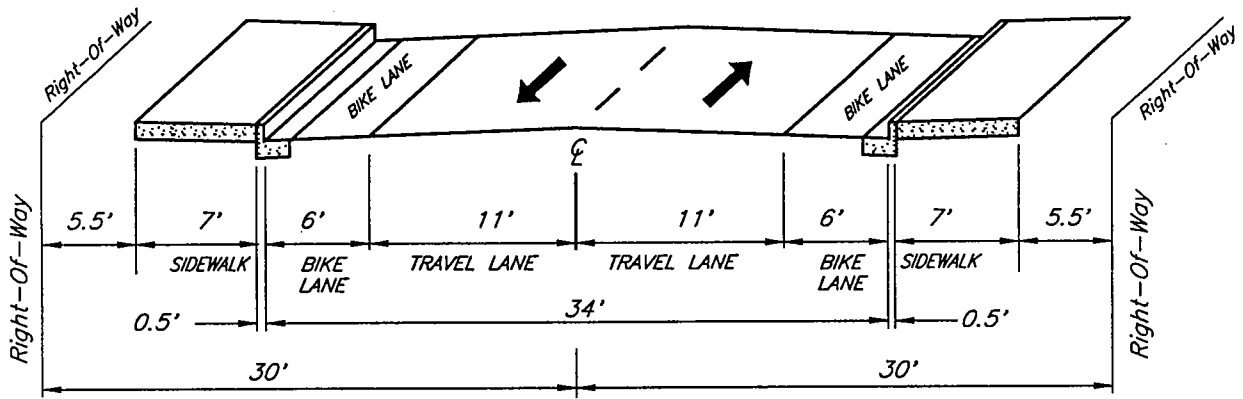
ATTACHMENT 1  
PROJECT VICINITY MAP



ATTACHMENT 2

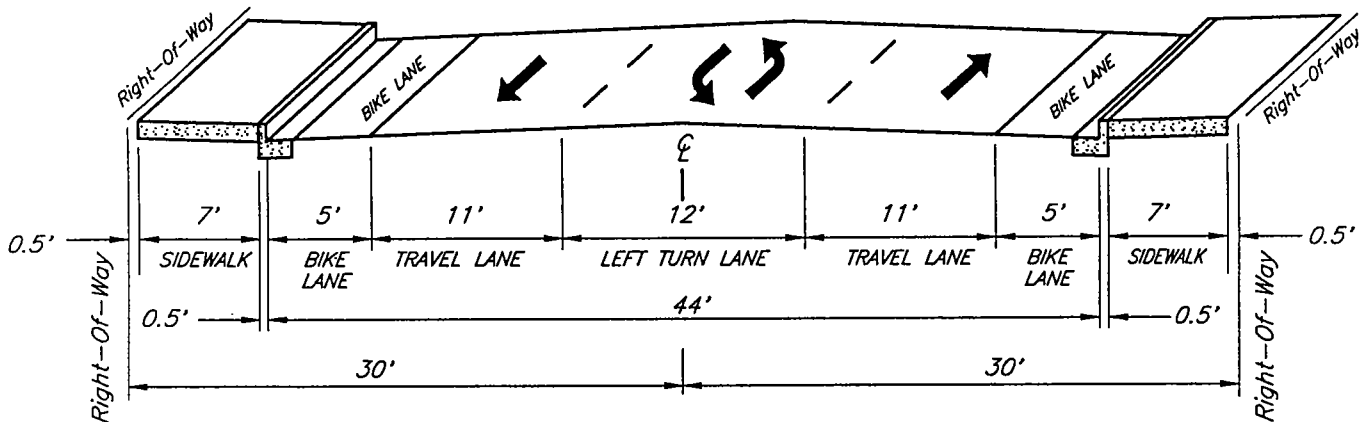


2-LANE AND 3-LANE ROADWAY TYPICAL SECTIONS



2-LANE URBAN ROADWAY

North 19th Street Between Hayden Bridge Road and Yolanda Avenue. Yolanda Ave. Between North 19th and 23rd Streets.



3-LANE URBAN ROADWAY

North 19th Street Between the Springfield City Limits and Hayden Bridge Road.