

METROPOLITAN PLAN EXCEPTIONS

L. BAILEY HILL ROAD EXCEPTION

LEGEND

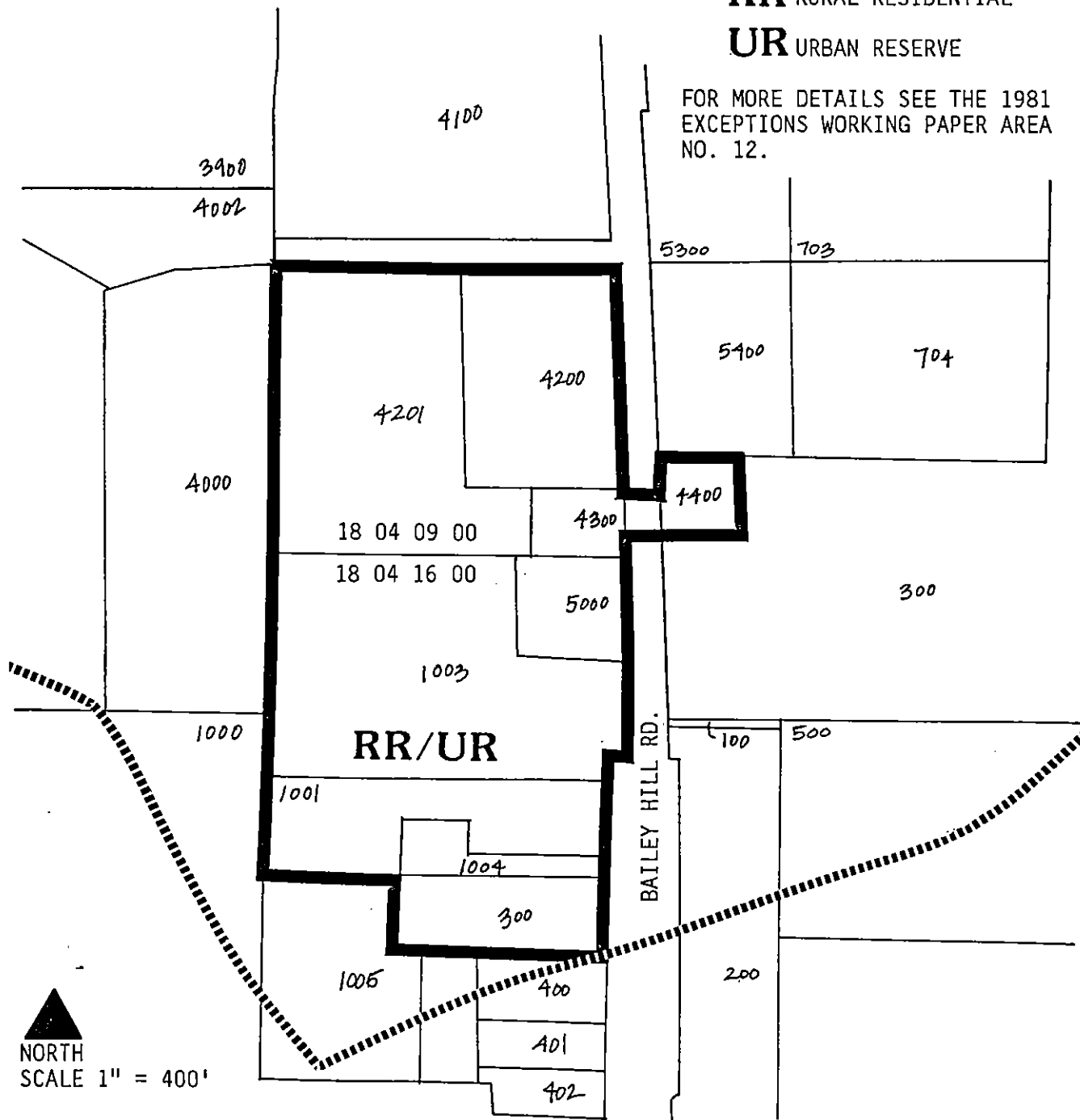
█ EXCEPTION AREA

⋯ PLAN BOUNDARY

RR RURAL RESIDENTIAL

UR URBAN RESERVE

FOR MORE DETAILS SEE THE 1981
EXCEPTIONS WORKING PAPER AREA
NO. 12.



METROPOLITAN PLAN EXCEPTIONS

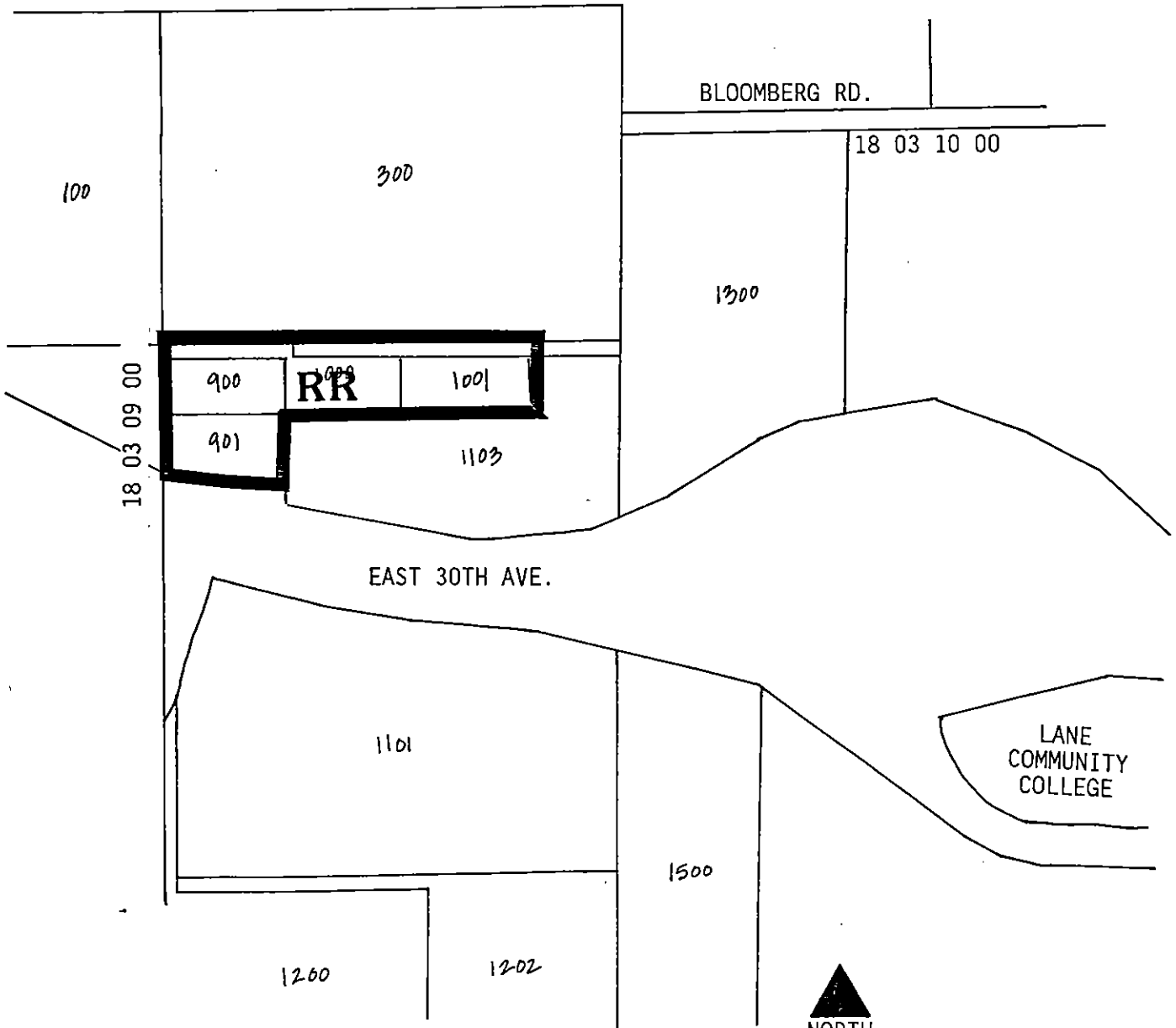
M. BLOOMBERG ROAD EXCEPTION

LEGEND

■ EXCEPTIONAL AREA

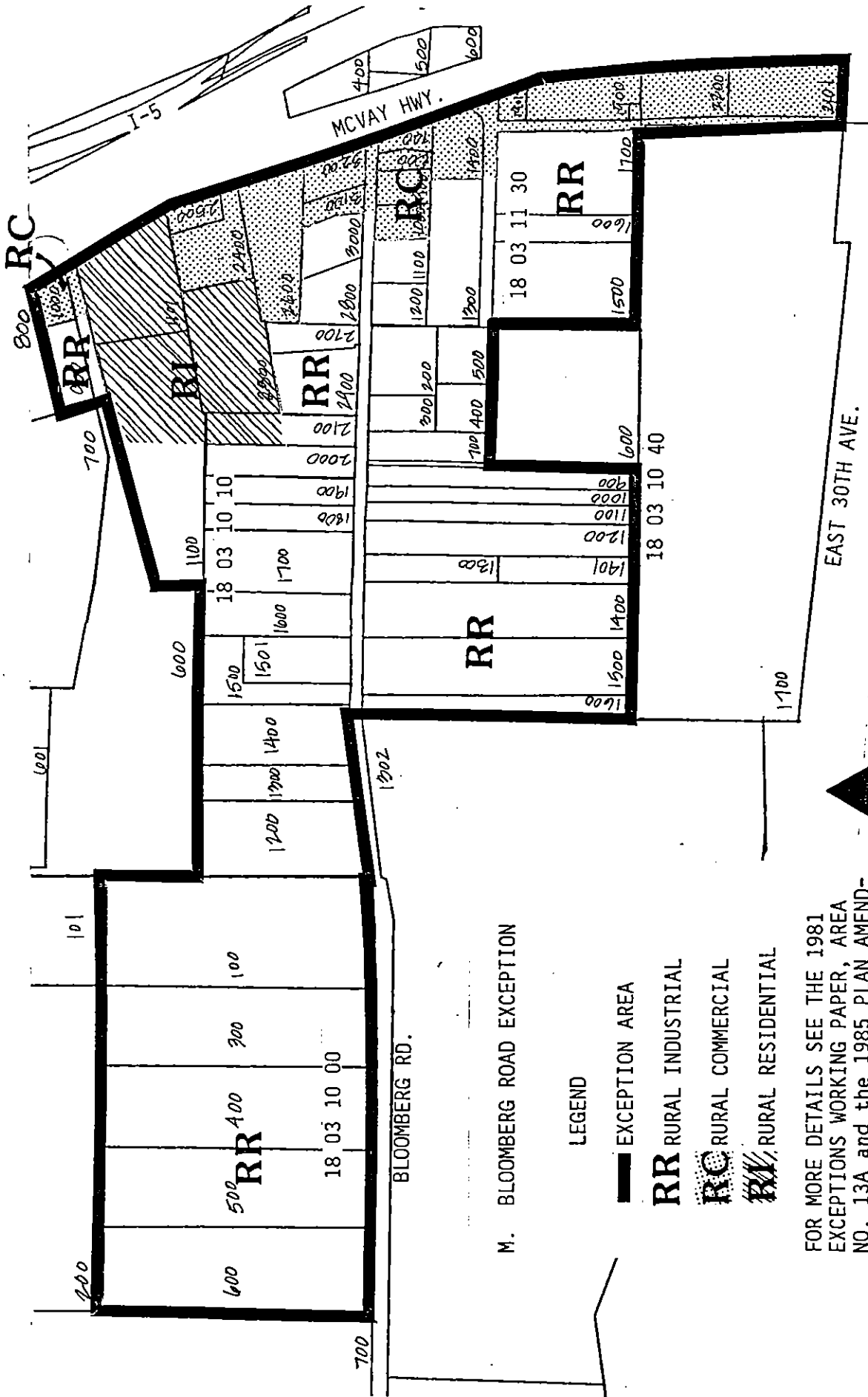
RR RURAL RESIDENTIAL

FOR MORE DETAILS SEE THE 1981
EXCEPTIONS WORKING PAPER, AREA
NO. 13B



▲
NORTH
SCALE 1" = 400'

METROPOLITAN PLAN EXCEPTIONS



M. BLOOMBERG ROAD EXCEPTION

LEGEND

- EXCEPTION AREA
- RR RURAL INDUSTRIAL
- RC RURAL COMMERCIAL
- RR RURAL RESIDENTIAL

FOR MORE DETAILS SEE THE 1981 EXCEPTIONS WORKING PAPER, AREA NO. 13A and the 1985 PLAN AMENDMENT LANE COUNTY ORDINANCE NO. PA 893



METROPOLITAN PLAN EXCEPTIONS

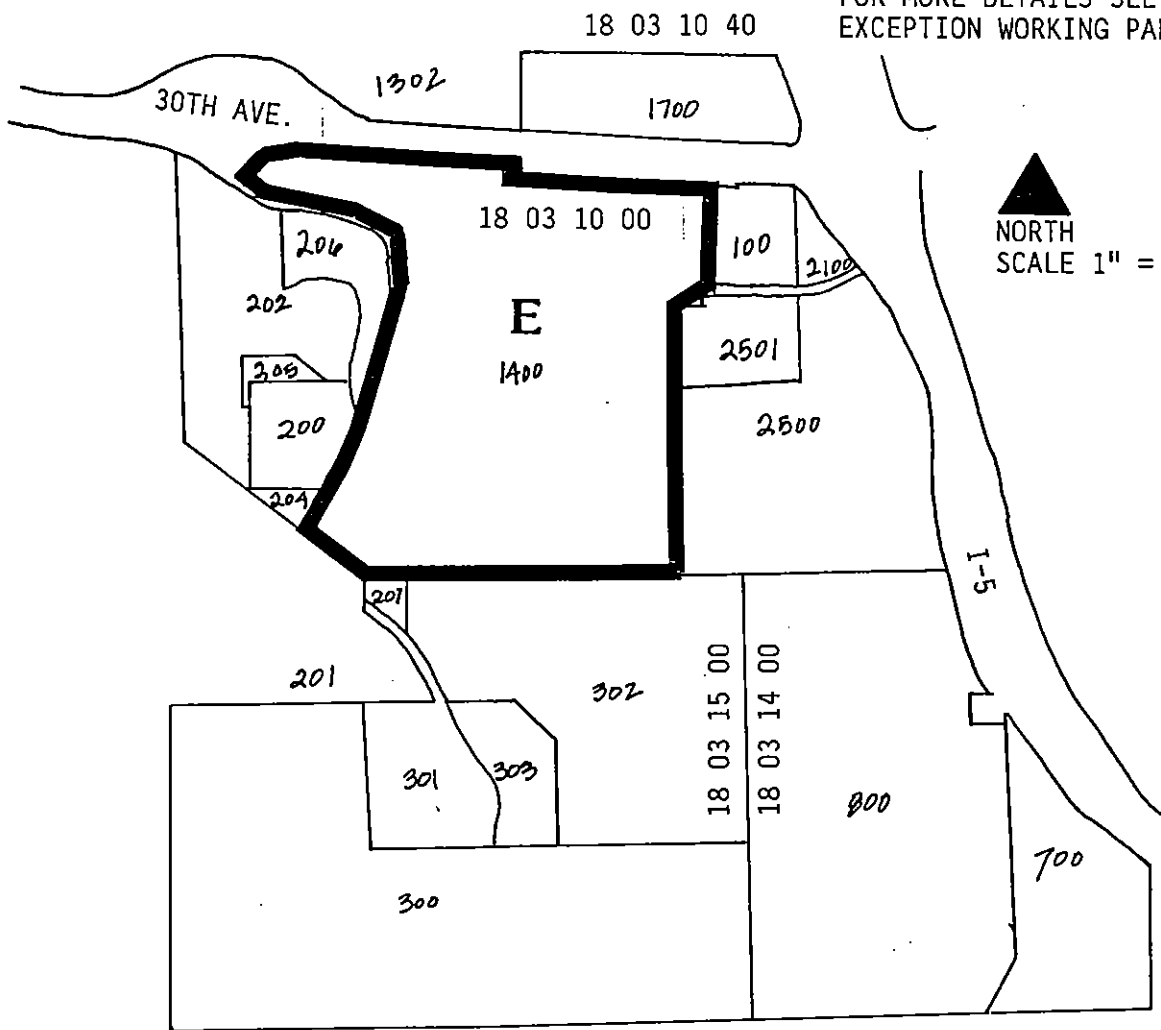
O. LANE COMMUNITY COLLEGE EXCEPTION

LEGEND

█ EXCEPTION AREA

E GOVERNMENT AND EDUCATION

FOR MORE DETAILS SEE THE 1981
EXCEPTION WORKING PAPER.



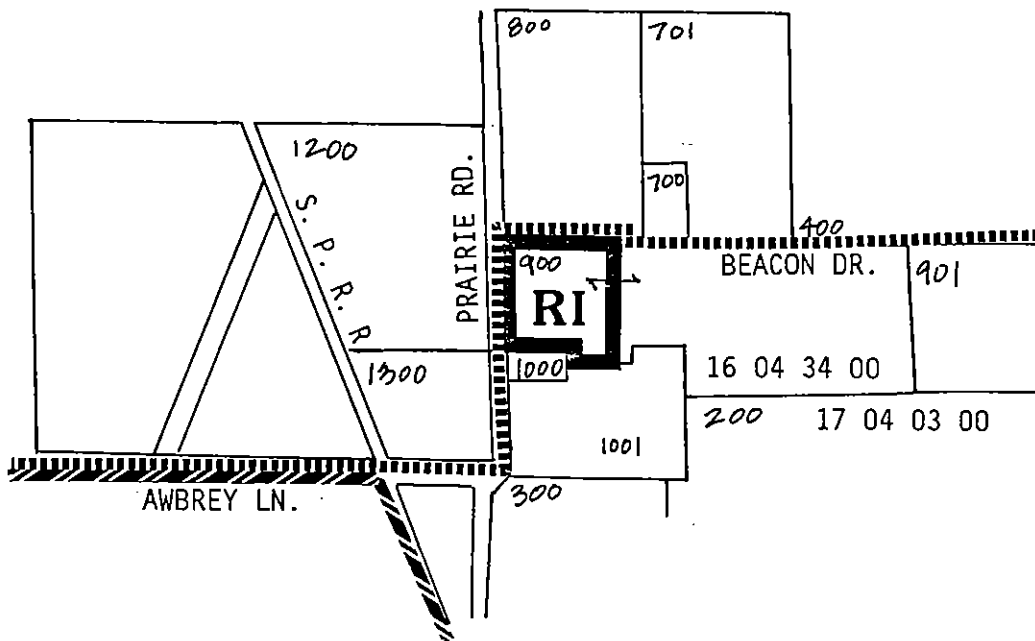
METROPOLITAN PLAN EXCEPTIONS

P. KNIGHT TRUCKING EXCEPTION

LEGEND

- EXCEPTION AREA
- ▨ URBAN GROWTH BOUNDARY
- ⋯ PLAN BOUNDARY
- RI** RURAL INDUSTRIAL

FOR MORE DETAILS SEE THE 1984 EXCEPTIONS WORKING PAPER AND THE 1985 PLAN AMENDMENT, LANE COUNTY ORDINANCE NO. PA893.



▲
NORTH
SCALE 1" = 1425'

METROPOLITAN PLAN EXCEPTIONS

Q. KOKKELER ESTATES EXCEPTION

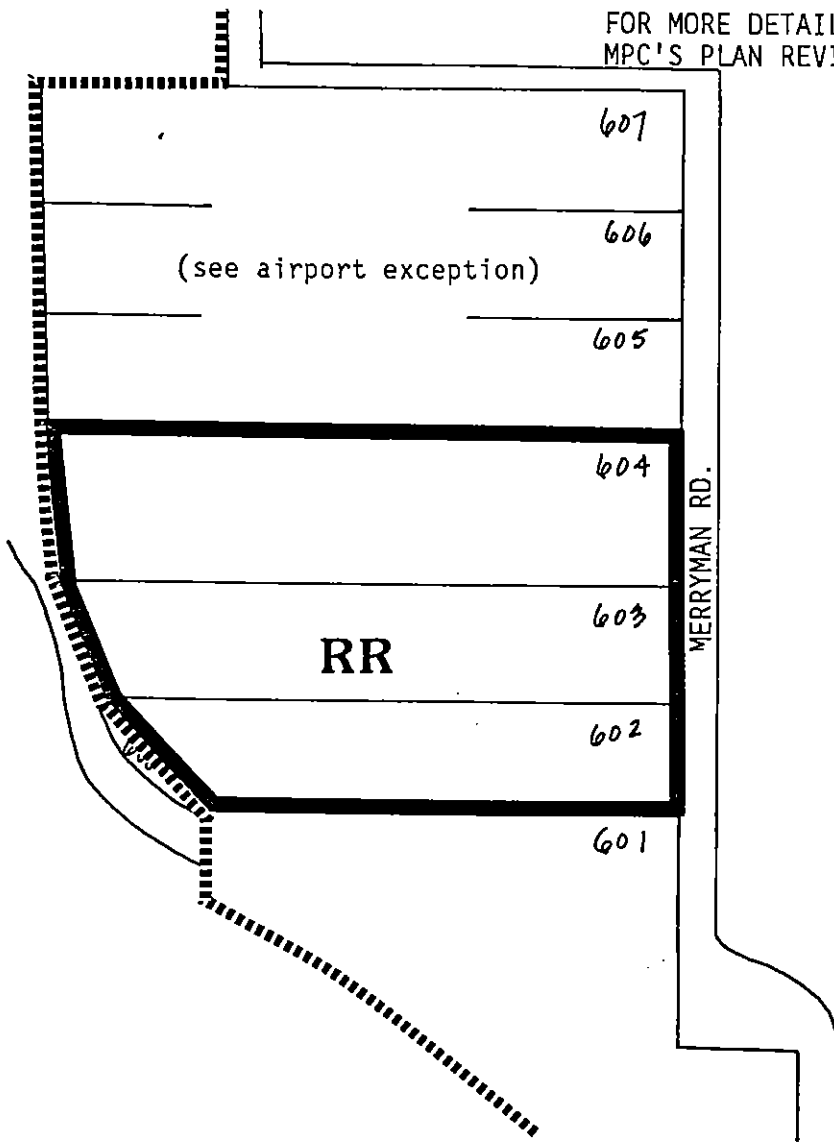
LEGEND

— EXCEPTION AREA

..... PLAN BOUNDARY

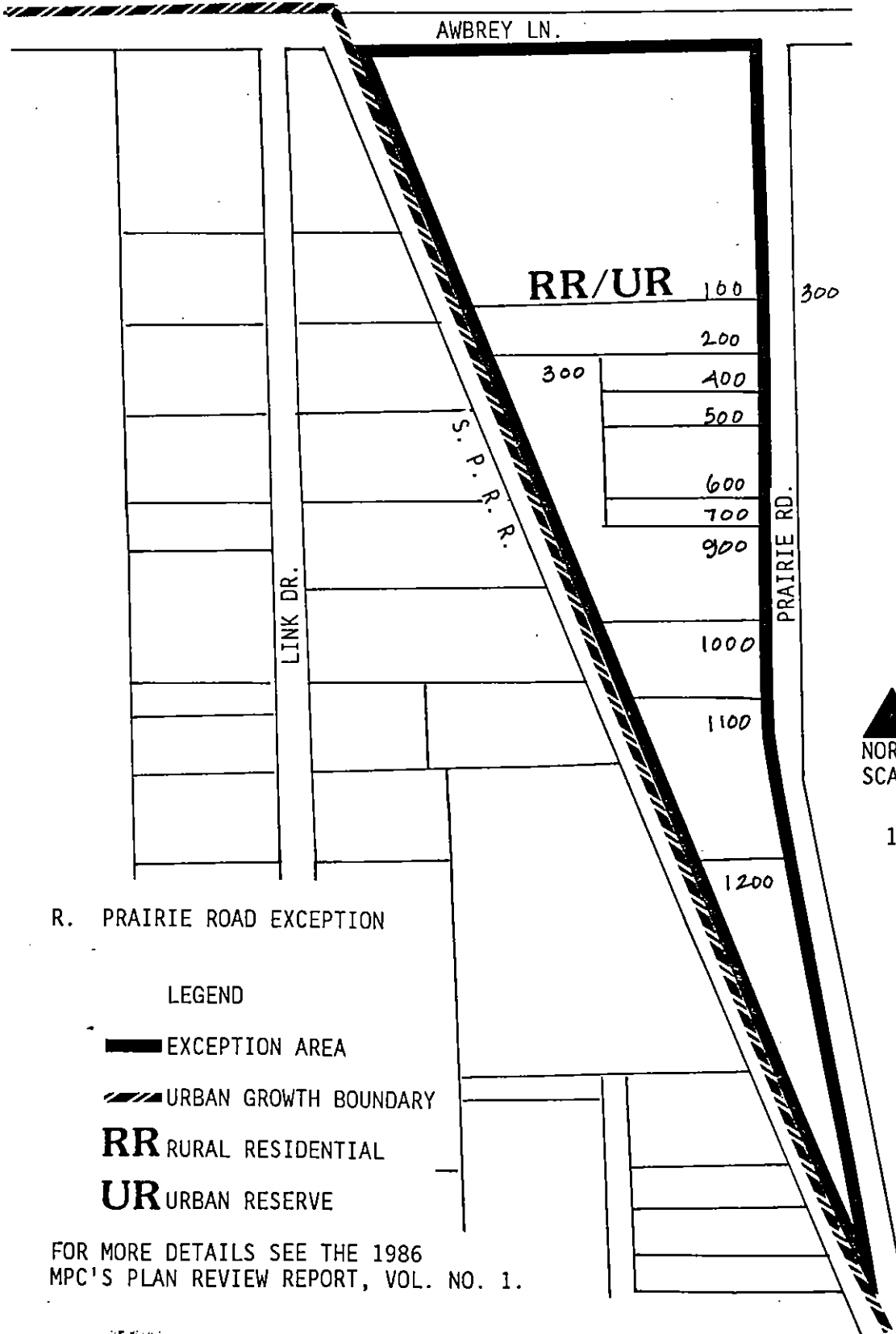
RR RURAL RESIDENTIAL

FOR MORE DETAILS SEE THE 1986
MPC'S PLAN REVIEW REPORT VOL. NO. 1



▲
NORTH
SCALE 1" = 100'

METROPOLITAN PLAN EXCEPTIONS



▲
NORTH
SCALE 1" = 100'

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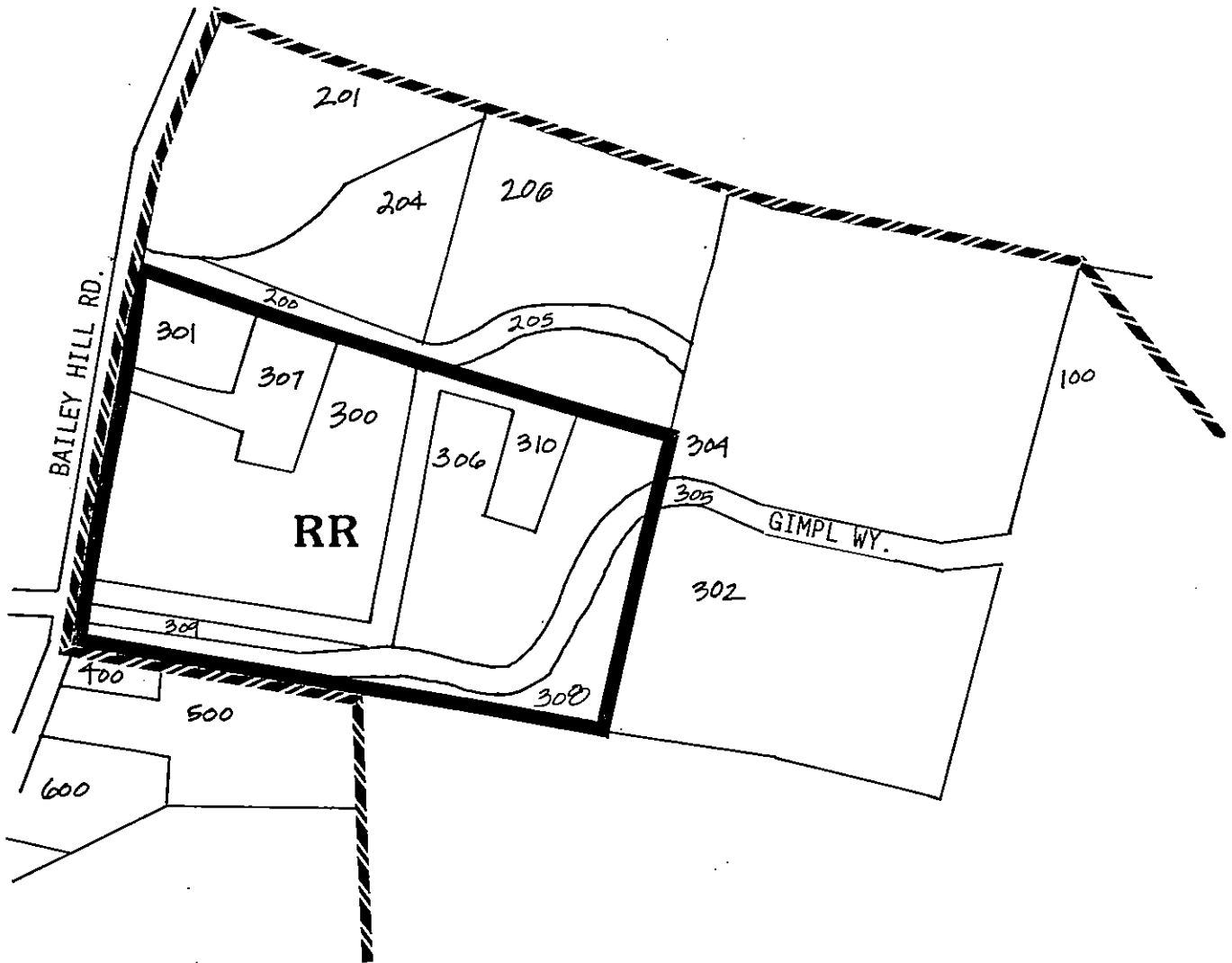
R. PRAIRIE ROAD EXCEPTION

LEGEND

- EXCEPTION AREA
- ▨ URBAN GROWTH BOUNDARY
- RR** RURAL RESIDENTIAL
- UR** URBAN RESERVE

FOR MORE DETAILS SEE THE 1986
MPC'S PLAN REVIEW REPORT, VOL. NO. 1.

METROPOLITAN PLAN EXCEPTIONS



S. BAILEY HILL ROAD EXCEPTION

LEGEND

█ EXCEPTION AREA

▨ URBAN GROWTH BOUNDARY

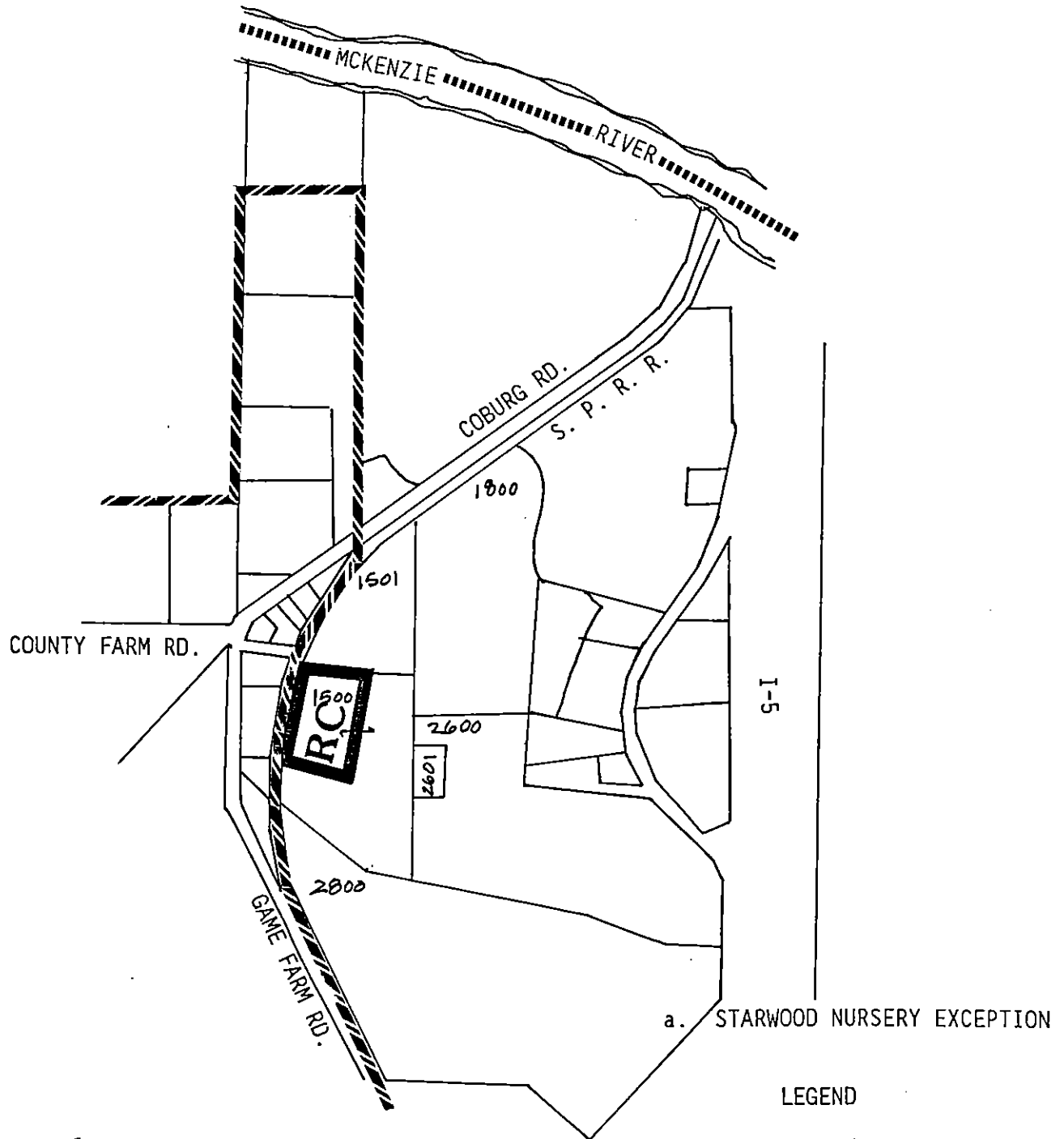
RR RURAL RESIDENTIAL

▲
NORTH
SCALE 1" = 100'

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FOR MORE DETAILS SEE the 1986
MPC'S PLAN REVIEW REPORT, VOL. NO. 1.


METROPOLITAN PLAN EXCEPTIONS



a. STARWOOD NURSERY EXCEPTION

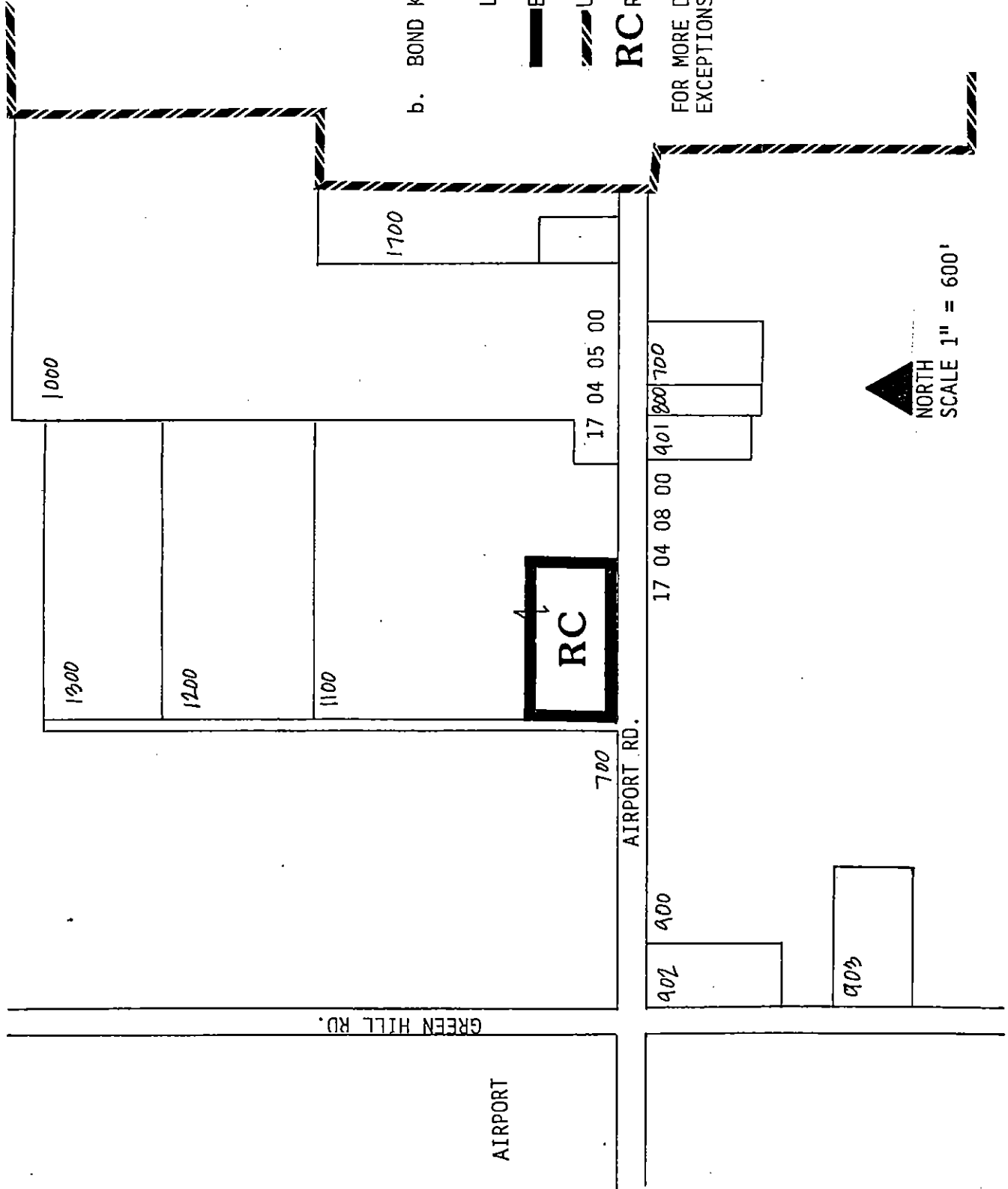
LEGEND

-  EXCEPTION AREA
-  URBAN GROWTH BOUNDARY
-  PLAN BOUNDARY
- RC** RURAL COMMERCIAL


 NORTH
 SCALE 1" = 800'

FOR MORE DETAILS SEE THE 1984 EXCEPTIONS WORKING PAPER.

METROPOLITAN PLAN EXCEPTIONS



b. BOND KENNEL EXCEPTION

LEGEND

— EXCEPTION AREA

▨ URBAN GROWTH BOUNDARY

RC RURAL COMMERCIAL

FOR MORE DETAILS SEE THE 1984 EXCEPTIONS WORKING PAPER

▲ NORTH
SCALE 1" = 600'

APPENDIX D

APPENDIX D

AUXILIARY MAPS

In addition to the urban growth boundary, other factors are important as they interface with land use patterns. They are illustrated on the following Auxiliary Maps and on the Public Facilities Plan project maps contained in Appendix A. The Auxiliary Maps include major public facilities and areas which affect urban development. For transportation, storm and sanitary sewer, and water facilities, refer to Appendix A. The general location of facilities shown on the Auxiliary Maps is information which supports the Metropolitan Plan but does not establish public policy. The general location of the facilities shown on these small scale maps are not intended to reflect the precise configuration or location of facilities which will be determined through additional planning, engineering studies, project design, or environmental review.

The following facilities and subjects will be shown on the Auxiliary Maps in Appendix D:

- fire station locations
- solid waste site
- electrical substations and transmission lines
- airport zones
- urban growth boundary
- Willamette River Greenway boundaries
- schools
- parks

Where amendments to the existing Auxiliary Maps in the Plan have been adopted, those amendments will be reflected on the maps in this Appendix.

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EXHIBIT 'A'

Ordinance No. PA 931

This exhibit contains amendments in legislative form: additions and [deletions].

REVISED May 6, 1987

Page I-1, Amend the second paragraph under "B. Purpose", as follows:

The Plan is intended to designate a sufficient amount of urbanizable land to accommodate the need for further urban expansion, taking into account the growth policy of the area to accommodate a population of 293,700. The Plan also identifies the major public facilities required to meet the land use needs designated within the urban growth boundary.

Page I-2, amend the list by adding a new #11, as follows:

11. Identifies the major transportation, sanitary and storm sewer and water projects needed to serve a future population of 293,700.

Page I-3, Add a new section titled "Appendices", and include the following language:

There are four appendices to this Plan:

Appendix A Public Facility Plan Project Lists and Maps for Water, Storm Sewers, Sanitary Sewers and Transportation

Appendix B List of Refinement and Functional Plans and Map of Refinement Plan Boundaries

Appendix C List of Exceptions and Maps of Site Specific Exception Area Boundaries

Appendix D Auxiliary Maps Showing the Following:
fire station locations
solid waste site
electrical substations and transmission lines
airport zones
urban growth boundary
Greenway boundary
schools
parks

Page I-4, "E. Relationship to Other Plans and Policies", amend the first paragraph as follows:

"...and it is important that it be [augmented] supplemented by more detailed refinement plans....."

Page I-7, Section H amend the title and substance of of this section as follows:

"Relationship to the Technical Supplement, [and] Working Papers, and Public Facilities Plan Technical Report".

Combine paragraphs two and three of this section.

Page I-7, amend paragraph 3 as follows:

These findings, in turn, formed an important share of the basis for [updating] the goals, objectives, and [recommendations of each of the 1990 General Plan elements] policies in this Plan.

Page I-7, add a new paragraph, as follows:

The Public Facilities Plan Technical Report also serves as a background information document supporting the Metropolitan Plan. The only portions of the Public Facilities Plan which establish public policy are incorporated in Metropolitan Plan policy and in Appendix A.

Page I-9, "General Findings", amend the second sentence of finding #2 as follows:

2. Such coordination can be enhanced through use of the Public Facilities Plan and scheduling of priorities.

Section II-E, move Section 8 "Exceptions" list (1984 and 1985 Plan Amendments) to Appendix C, and add the following sentence:

The list of exceptions and site specific maps, which are amendments to the Plan, are contained in Appendix C.

Page II-E-11, Amended Section 8, "Exceptions" list, (1984 and 1985 Plan Amendments) should be amended to list another exception. This amendment to the "Exceptions" list would be relocated in Appendix C along with all the exceptions:

Other exceptions

1. 6th-7th Avenue Extension from the urban growth boundary westward to the Plan Boundary.

Page II-E-15 - Delete section #14. As part of the integration of the Public Facilities Plan into the Metropolitan Plan and as part of the Mid-Period Review, it was determined that future transportation facilities, like the other public facilities, would not be shown on the Plan diagram.

[14. Major Transportation Corridors and Other Public Facilities

Recognizing the close interrelationship between transportation facilities and land use, the major existing and planned streets and highways included in the adopted Eugene-Springfield Area 2000 Transportation Plan are shown on the Plan diagram.]

Page II-E-16, "Plan Diagram Auxiliary Maps", move this section to Appendix D and amend this section as follows:

In addition to the urban growth boundary, other factors are important as they interface with land use patterns. They are illustrated on the [four] following "Plan Diagram Auxiliary Maps" and the Public Facility Plan project maps contained in Appendix A. [and they] The auxiliary maps include major [utility corridors, sewer trunk lines, electrical substations, the boundaries of the Willamette River Greenway, and identification of areas subject to neighborhood refinement or community plans adopted subsequent to adoption of the 1990 Plan in 1972. Areas for which refinement plans are in process but not adopted in March 1982, are also shown on Auxiliary Map 4] public facilities and areas which affect urban development. For transportation, storm and sanitary sewer, water

facilities, refer to Appendix A. The general location of facilities shown on the auxiliary maps is information which supports the Metropolitan Plan but does not establish public policy. The general locations of the facilities shown on these small scale maps are not intended to reflect the precise configuration or location of facilities which will be determined through additional planning, engineering studies, project design, or environmental review.

Move Auxiliary Map 4 to Appendix B. The list of refinement and functional plans which support the Metropolitan Plan and the accompanying map showing refinement plan boundaries will be included in Appendix B.

Delete the language describing the Street and Highway Network from the auxiliary maps.

Items that will remain on the auxiliary maps in the appendices after PFP information is removed, include:

- fire station location
- solid waste site
- electrical substations and transmission lines
- airport zones
- Urban Growth Boundary
- Greenway boundary
- schools
- parks
- refinement plan boundaries in Appendix B

Page II-E-23 Delete the reference to future streets and highways from the Plan diagram.

Page III-B-5 Amend policy #15 as follows:

15. Encourage the development of transportation facilities which would improve access to industrial and commercial areas and improve freight movement capabilities by implementing the policies and [recommended] projects in the Eugene-Springfield Metropolitan Area [2000] Transportation Plan (TransPlan) and the Mahlon Sweet Field Master Plan, as outlined in Chapter 8(a), "On Airport Land Use."

Page III-C-9 Amend policy #19, as follows:

19. Local governments shall develop policies and local controls for protection and management of wetland areas by the completion of the next Metro Plan Update.

Page III-C-11 Add a new policy #37 as follows:

37. Prior to the completion of the next Plan Update, the air, water and land resource quality of the metropolitan area will be reassessed.

Page III-C-11 Add a new policy #38 as follows:

38. A metropolitan-wide inventory of noise sources and complaints shall be addressed during each Plan Update and Periodic Review process.

Page III-C-11 Add a new policy #39 as follows:

39. Prior to the completion of the next Plan Update, the Goodpasture Island Great Blue Heronry shall be inventoried and assessed for its significance as a Goal 5 resource. A Goal 5 ESEE analysis shall be conducted, if required.

Page III-D-5 Add a new policy #13 as follows:

13. The taking of an exception shall be required if a non-water-dependent transportation facility requires placing of fill within the Willamette River Greenway setback.

Page III-F-1 Amend the second paragraph as follows:

The Eugene-Springfield Metropolitan Area [2000] Transportation Plan (TransPlan) forms the basis for the surface transportation part of this element in describing future needs for people and goods movement. Projections of increased population and employment were translated into trips through the transportation planning process. This forecasted demand and other considerations, such as safety, served as the rationale for the policies, facilities, and services contained in TransPlan [the Transportation Plan]. TransPlan [The Transportation Plan] also outlines specific transportation goals, objectives, and policies [, and recommendations] necessary to meet metropolitan transportation needs for a population of 293,700 [through the year 2000].

Page III-F-1 Replace the second paragraph with the following:

[The Eugene-Springfield Metropolitan Bikeway Master Plan serves as the basis for the metropolitan bikeway needs in the area. Originally adopted in 1975, this plan needs to be updated to reflect the substantial bikeway improvements made since 1975, to resolve some conflicts with the Transportation Plan, and to respond to continued community interest in bicycling.]

TransPlan contains a metropolitan scale bicycle element which replaces the 1975 Eugene-Springfield Metropolitan Bikeway Master Plan.

Page III-F-2 The following findings should be amended to reflect changes to T-2000, the Bikeway Master Plan and the emphasis on a population level rather than a future year:Findings:

12. A number of the policies contained in the Eugene-Springfield Metropolitan Area [2000] Transportation Plan (TransPlan) are directed toward increasing the efficiency of existing transportation systems.
15. Local [and metropolitan] bikeway plans have been adopted which outline the future needs of the metropolitan area. Metropolitan bikeway needs are contained in TransPlan.
17. Current levels of local, state, and federal funding are inadequate to meet the surface transportation needs forecasted for the future population of 293,700. [2000.]

18. TransPlan meets all the State public facility planning requirements with the exception of a general condition assessment which is contained in the Public Facilities Plan Technical Report.

Renumber the remaining findings accordingly.

Page III-F-3 Amend finding #22 to be consistent with Lane County's Rural Comprehensive Plan:

22. In addition to the General Plan and the Mahlon Sweet Field Master Plan, Lane County's General [Willamette-Long Tom Subarea] Plan affects the land uses within the airport environs (within approximately five miles of the airport proper).

Page III-F-4 Amend policy #1 to delete reference to T-2000 and the Bikeway Master Plan and replace with reference to TransPlan:

1. The goals, objectives policies, [recommendations,] and [proposed] facilities and services contained in the adopted Eugene-Springfield Metropolitan Area [2000] Transportation Plan (TransPlan) [and the Eugene-Springfield Metropolitan Bikeway Master Plan] shall serve as the basis for guiding surface transportation improvements in the metropolitan area. TransPlan is adopted as public policy by reference in this Plan, but specifically excluded as public policy are the following: phasing, cost estimates, and project justification contained in the project lists and the Financial Section (IX). Those parts of TransPlan are informational only and are not adopted as policy in TransPlan or by reference in this Plan.

Page III-F-5 Amend policy #6 to delete reference to T-2000 and replace it with reference to TransPlan:

6. Because it is recognized [,] that the Eugene-Springfield Metropolitan Area [2000] Transportation Plan (TransPlan) is a component of this Metropolitan Area General Plan, TransPlan [the Transportation Plan] should be evaluated and revised, if necessary, to maintain consistency and assure transportation facilities are available to serve land uses included in the Metropolitan Area General Plan.

Page III-F-6 Amend policy #15 as follows:

15. The City of Eugene and Lane County shall [adopt] maintain up-to-date airport safety ordinances which regulate the height of structures, electromagnetic and/or visual interference with aircraft, and other hazards to airport safety.

Page III-G-2 Delete the final sentence from the second paragraph, and add the following paragraphs:

In planning and programming for public utilities, services, and facilities, present and near future needs of the metropolitan area should be met in a coordinated manner, recognizing the long-term, ultimate needs and service area. This metropolitan-wide cooperation is reflected in the State mandated Public Facilities Plan. Major public facilities from the Public Facilities Plan are incorporated as Plan policy in Appendix A.

Generally, construction of projects is based upon the phasing portion of the Public Facilities Plan, but actual decisions on timing and financing are controlled solely by the capital improvements programming and budget processes of individual jurisdictions.

Amendments to either the project lists or maps in Appendix A are amendments to this Plan and require simultaneous amendments to this Plan and to affected functional plans. Changes to the phasing, cost estimates, and project justification will be made from time to time in conjunction with the semi-annual amendment and update processes; those changes can be made through the budgeting and capital improvement processes, and do not necessitate amendments to TransPlan or the Metropolitan Plan. Because the Public Facilities Plan Technical Report is a background document and all public policy aspects are incorporated directly into the Metropolitan Plan, changes to the Public Facilities Plan Technical Report can occur at a later time during semi-annual amendment and update processes.

Page III-G-3 Add a new finding #18 as follows:

18. State law requires development of a Public Facilities Plan to coordinate implementation of planned water, sanitary sewer, storm sewer and transportation projects.

Page III-G-6 Add a new policy #18 as follows:

18. The water, sanitary and storm sewer sections of the Metropolitan Public Facilities Plan shall serve as the basis for guiding water, sanitary and storm sewer improvements in the metropolitan region.

Page III-G-6 Add a new policy #19 as follows:

19. Additions to or deletions from the project list or significant change to project location requires amending the Public Facilities Plan.

Page III-G-6 Add a new policy #20 as follows:

20. Changes to project phasing schedules or anticipated costs and financing shall be made in accordance with budgeting and capital improvement program procedures of the affected jurisdiction(s).

Page III-G-6 Add a new policy #21 as follows:

21. Project timing and financing modifications do not require amendment of the Public Facilities Plan. Modifications should be reflected in the Public Facilities Plan at the next regularly scheduled update.

Page III-G-6 Add a new policy #22 as follows:

22. Both timing and financing provisions for public facilities are not considered land use decisions, and therefore, cannot be the basis of appeal in accordance with State law.

Page III-G-6 Add a new policy #23 as follows:

23. Prior to the completion of the next Plan Update, the Lane County Solid Waste Management Plan shall be revised to reflect the requirements of the Recycling Opportunity Act and changes in the inventory of solid waste sources and sites.

Page III-H-5 Add a new policy #7 as follows:

7. The City of Eugene shall cooperate with the University of Oregon in the resolution of any loss of recreational facilities associated with development in the Riverfront Park.

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